



12-14 Day Street, East Maitland

STATEMENT OF HERITAGE IMPACT
DEMOLITION AND PROPOSED DEVELOPMENT

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1. INTRODUCTION

- a. Jason Penhall, Heritage Consultant for Contemporary Heritage has been engaged to prepare a Statement of Heritage Impact for 12-14 Day Street, East Maitland, Lot/Section/Plan no: 1/-/DP742762.
- b. This report adopts the methodology outlined in the Heritage Assessments and Statements of Heritage Impact (prepared by the Heritage Office and the Department of Urban Affairs and Planning, 1996.) It has been undertaken in accordance with the principles of the Burra Charter, 2013.
- c. The subject site is located within the East Maitland Heritage Conservation Area. There are a number of heritage items in the vicinity, notably the State significant East Maitland Railway Station to the immediate northeast of the site and East Maitland Post Office to the southeast.
- d. The site is accessed off Day Street and is within an isolated R1 General Residential lot of land, with B4 Mixed Use land to the northwest, RE1 Public Recreation land to the southeast and SP2 Infrastructure to the north-eastern railway land.
- e. This report was prepared by CONTEMPORARY HERITAGE.

Nominated Architect: Jason Penhall

BA(Hons.) Arch., PG-Cert. Arch., M.Arch., PG-Dip. Prof. Prac.

Architect Reg. 11285



Figure 1 Site Location with Heritage Overlay

source: NSW Planning Portal - Subject site shown outlined in yellow

2. HISTORICAL CONTEXT

2.1 Historical Context

The original inhabitants of the area were the Wonnarua Nation (people of the mountains and plains). The Wonnarua people are members of the Mindaribba Local Aboriginal Land Council that covers the traditional lands of more than 2 thousand hectares including Maitland, Cessnock, Singleton, Dungog and Port Stephens.



Figure 2 1853 Frederick Terry (1827 – 1869)

View of East Maitland

source: University of Newcastle Living Histories

<https://livinghistories.newcastle.edu.au/nodes/view/101078>

Maitland was first settled by Europeans as a convict settlement between 1818 and 1821 and was called “The Camp” as well as Molly Morgan Plains and Wallis Plains.

Eventually, due to flooding problems in West Maitland, a second town was founded on higher ground and Major Sir Thomas Livingstone Mitchell surveyed and laid out a plan for East Maitland in 1829.

It is located on high ground to the east of Wallis Creek.

Both townships were defined in 1835; East Maitland became a municipality in 1862 and West Maitland in 1863. The city was formed in 1944 by uniting eight towns.¹

¹ Britannica

In the early 1800s and many historic buildings were built using local sandstone and cedar. Convict labour were cedar cutters and they built thatched huts in the brush. Maitland was originally covered by giant red cedars, fig trees, myrtle, softwood brush, gums and swamp oaks.

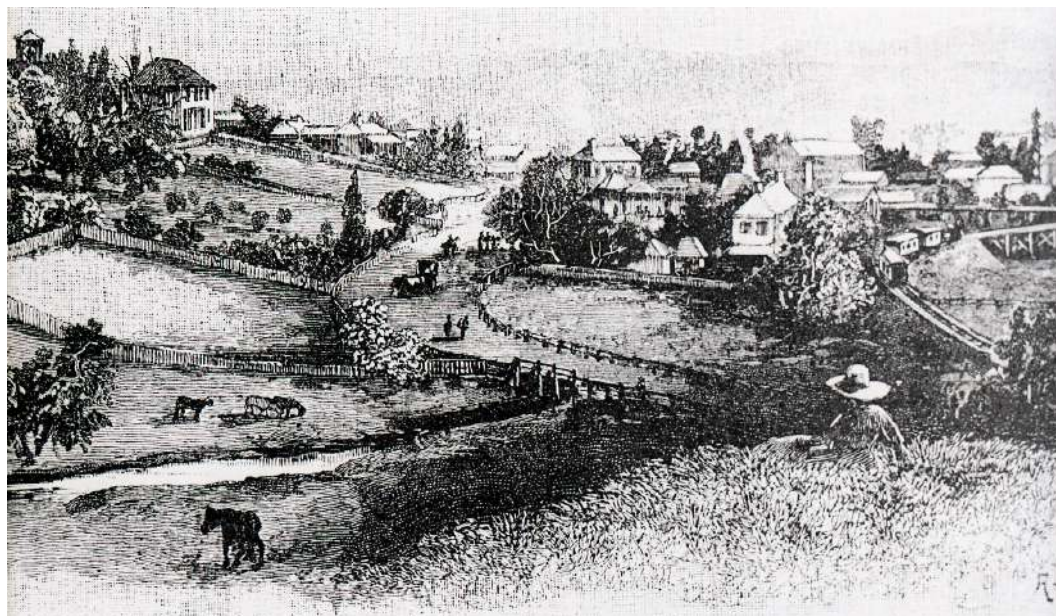


Figure 3 East Maitland in the 1850s, A H Fulwood, from Maitland 1863-1963
source: University of Newcastle Living Histories
<https://livinghistories.newcastle.edu.au/nodes/view/102654>

Maitland, built on the Hunter River, is one of the oldest regional centres in Australia and East Maitland is a suburb of the City of Maitland.

The full description can be found in the NSW Government Gazette but it was formed as follows using portions of:

- Simeon Lord's grant of 2,000 acres
- Geo. Brook's 2,000 acres
- John Hill Garvan's 640-acre grant
- John Callaghan's grants (two) of 640 acres
- to the north-east corner of James Weston's grant of 640 acres
- John Terry Hughes' grants (two) of 610 acres each
- A. W. Scott's 598 acres, parish of Hexham
- W. C. Wentworth's 1,070 acres, by a western boundary of W. C. Wentworth's 830 acres
- A. Sparke's grant of 300 acres,
- Edward Sparke's 2,000 acres,
- John Terry Hughes' grant of 300 acres;²

² NSW Government Gazette – Friday 29 December 1893, Pg 9765

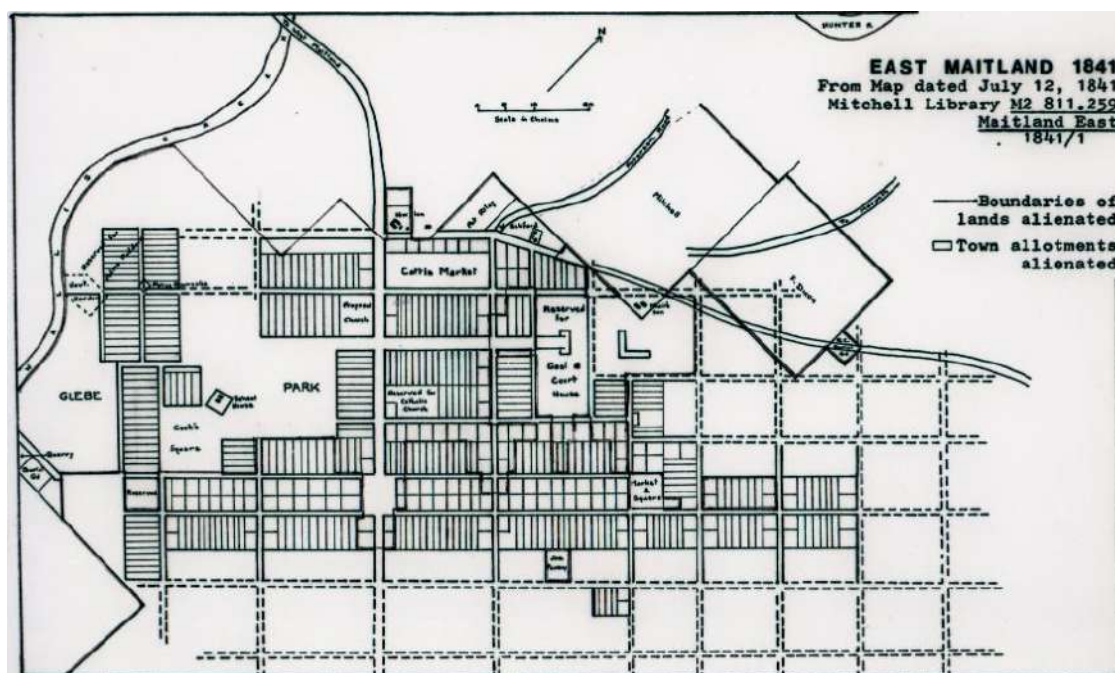


Figure 4 East Maitland East Maitland 1841
Mitchell Library M2 811.259/Maitland East/1841/1
redrawn D Bairstow MA Thesis, University of Sydney, 1980
source: University of Newcastle Living Histories – John Turner Collection
<https://livinghistories.newcastle.edu.au/nodes/view/101340>

The above image includes:

- Cattle market
- Reserved for Catholic Church
- Reserved for Gaol & Court House
- Market & Square
- Glebe Park
- Cook's Square
- School House
- RC Burial ground
- Police Barracks
- Government Gardens
- Quarry
- Land owned by:
 - James Pawboy
 - Mitchell
 - F. Dixon
 - Pat Riley
 - Eckford

2.2 12-14 Day Street History

The image below has been taken from the larger map "Plan of the Town of Maitland (East) : County of Northumberland, NSW, 1861" and shows the reserved land for the "Gaol and Courthouse" fronting Day Street.

The portion of land in which the subject site is located is outlined in green whilst the approximate location of "Roseneath" is pointed out with the green arrow:

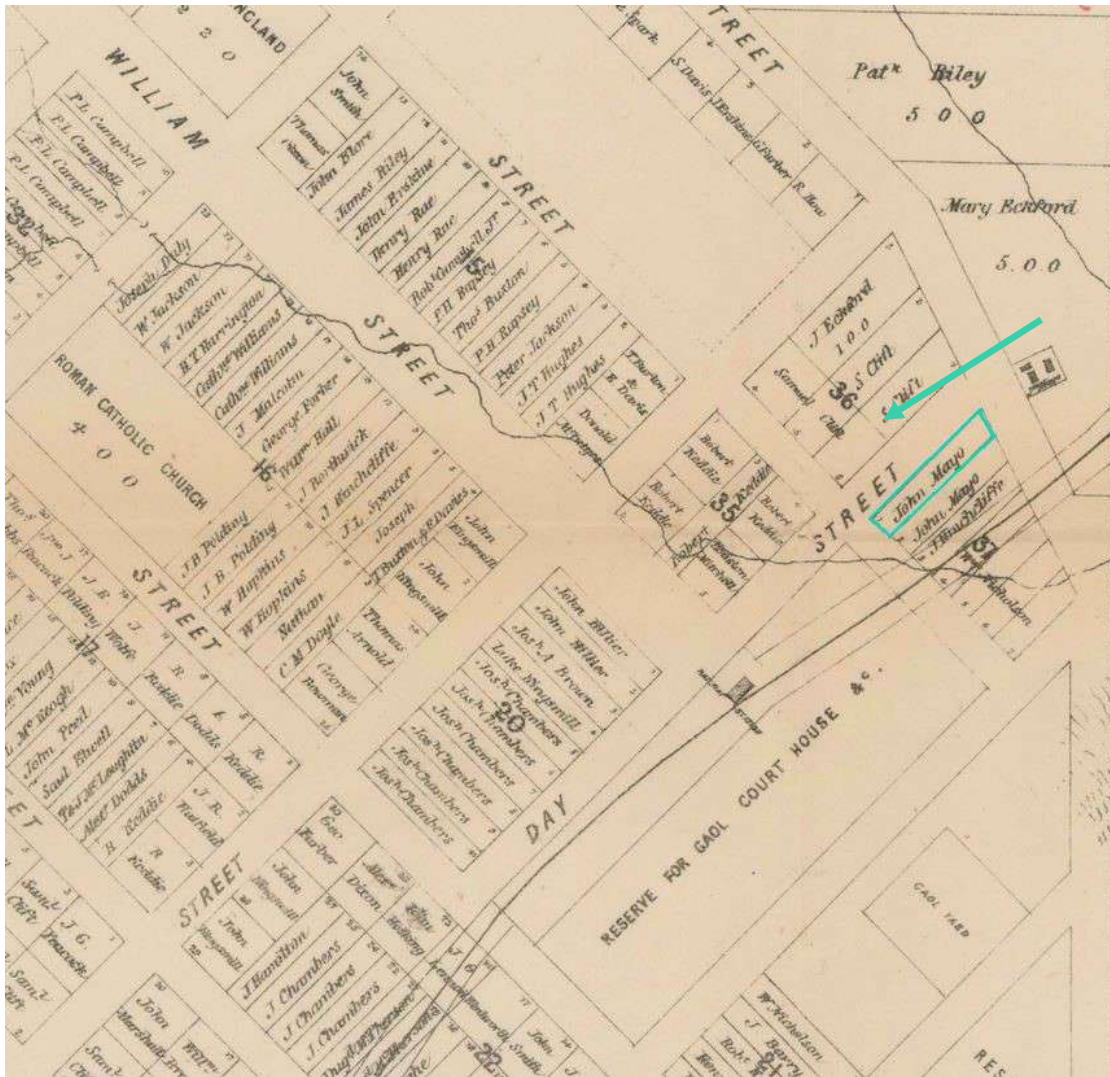
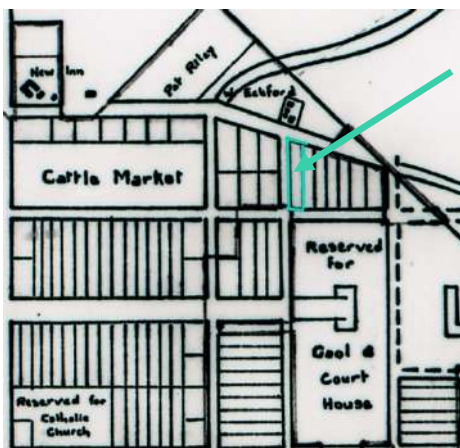


Figure 5 Plan of the Town of Maitland (East) : County of Northumberland, NSW, 1861
source: University of Newcastle Living Histories
<https://livinghistories.newcastle.edu.au/nodes/view/79037>



The extract to the left taken from the larger image earlier in the document shows the portion of land in which the subject site is located outlined in green and is the block later owned by John Mayo. The approximate location of the subject property, 10-12 Day Street, is pointed out with the green arrow:

The whole block was once owned by John Mayo and ex-convict who was transported in 1818 and sentenced to 7 years for the theft of a plated urn. Once he was listed as "free by servitude" he married ex-convict, Elizabeth Benson, in 1826 and in about 1828 he became a shoemaker and barber.



Figure 6 Hunter River Hotel, Melbourne Street, East Maitland
Former Maitland Wine Vaults 1924

source: Trove – Australian National University
<https://openresearch-repository.anu.edu.au/handle/1885/256317>

He committed a number of minor offences over the years but from 1838 John and Elizabeth ran the “Maitland Wine Vaults”, later known as the “Hunter River Hotel” and still located at 10 Day Street.³

John Mayo had his publican’s license renewed in 1842:

*Licenses renewed
John Mayo, the Maitland Wine Vaults⁴*



Figure 7 Hunter River Hotel, Maitland
source: Trove – Australian National University
<https://openresearch-repository.anu.edu.au/handle/1885/256317>

³ <https://convictrecords.com.au/convicts/mayo/john/94628>

⁴ The Sydney Herald Wed 27 Apr 1842 Page 2 <https://trove.nla.gov.au/newspaper/article/28652513>



The Hunter River Hotel was leased by John Madden with Tooth & Co. Limited in 1899 and rebuilt in 1955:

Figure 8 Documents, lease Hunter River Hotel Sydney 1899
source: trove – Powerhouse Collection
<https://collection.maas.museum/object/6387>



The subject property at 12-14 Day Street can just be seen to the immediate right of the image and pointed out with the green arrow:

Figure 9 Hunter River Hotel, Maitland
source: Trove – Australian National University
<https://openresearch-repository.anu.edu.au/handle/1885/123460>



Figure 10 Old house no. 12 Day Street, opposite Roseneath, East Maitland, NSW
The East Maitland Courthouse is visible in the background to image right and
East Maitland Railway Station in the background to image left.

source: University of Newcastle Living Histories - Athel D'Ombra Collection
<https://livinghistories.newcastle.edu.au/nodes/view/12623>

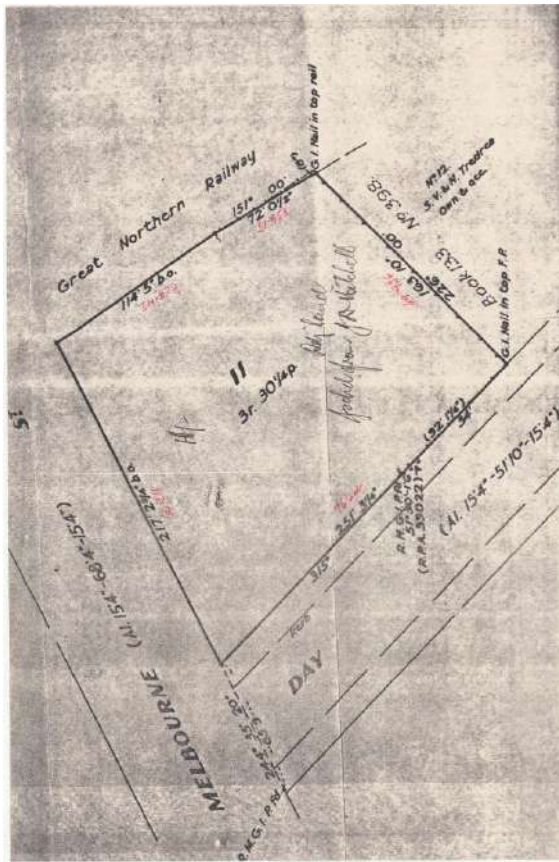
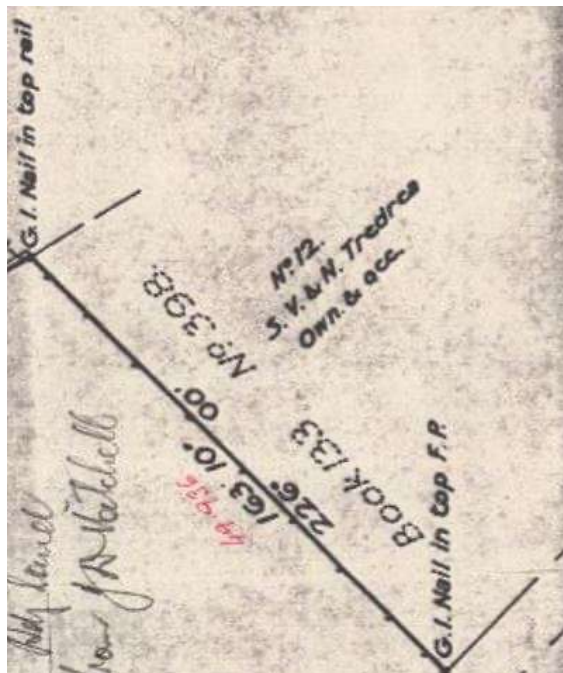


Figure 11 Architectural drawing for Hunter River Hotel East Maitland, New South Wales
source: trove – Powerhouse Collection
<https://collection.maas.museum/object/603552>



The extract to the left of the above image has the ownership of 12 Day Street being **S.V. & N. Tredres** - c.1950s due to the rebuild in 1955:

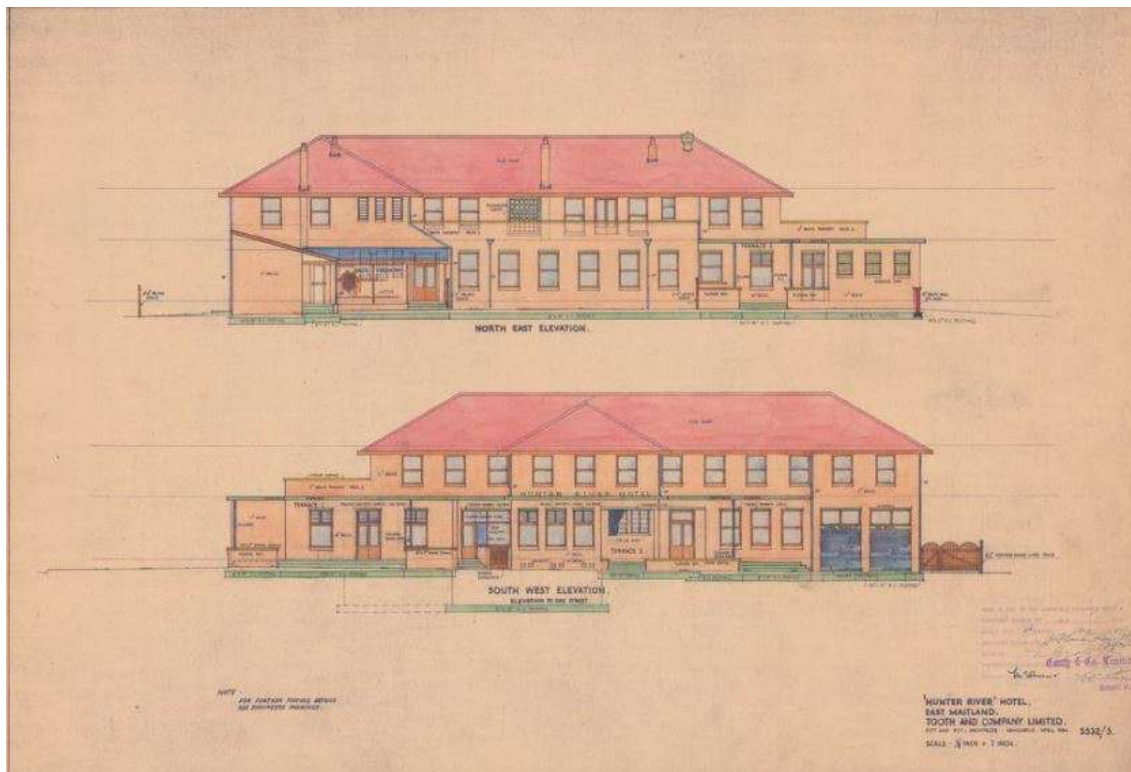


Figure 12 Architectural drawing for Hunter River Hotel East Maitland, New South Wales
source: trove – Powerhouse Collection - April 1954
<https://collection.maas.museum/object/603533>

2.3 Roseneath - 9 Day Street, East Maitland

"Roseneath", Day Street, East Maitland: A large two storied painted brick house on cellar foundations. The windows are twelved paned and shuttered, the doors are side-panelled. A feature of the upper and lower verandahs is the very finely turned balustrading. At the back is a semi-detached one storey building forming the usual courtyard with a well. This house was originally the Victoria Hotel and was built in about 1840. It has large handsome Victorian marble mantle-pieces in the lower rooms. It later became the home of Mr Percy Clift. Opposite is a duplex house known as the "Overflow." "Roseneath" was renowned for its lovely rose garden.⁵

As stated earlier in this document the property was originally owned by Mr George Clift and Mrs May Clift. An early advert shows Samuel Clift applied for a license for the "Victoria Inn" in 1946 The same article also shows John Mayo applying for a licence to the Hunter River Hotel:

ANNUAL LICENSING MEETING

John Mayo, Hunter River Hotel

Samuel Clift, Victoria Inn⁶

In 1857 it was named the "Victoria Hotel"

WANTED, a HORSEKEEPER, must be a steady sober man, of good character; also, a smart active BOY, about 13 or 14 years old, to work, amongst horses. Apply to MR. JOSEPH CLIFT, Victoria Hotel, East Maitland.⁷

The following image is an advert for a servant:

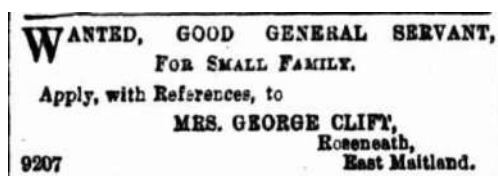


Figure 13 Roseneath – Advertisement

source: Trove - The Maitland Mercury and Hunter River General Advertiser Sat 20 Oct 1883
<http://trove.nla.gov.au/newspaper/article/908356>

⁵ HISTORICAL BUILDINGS OF MAITLAND AND DISTRICT – Roseneath

<https://www.flickr.com/photos/98887654@N05/51004626461/in/photostream/>

⁶ The Maitland Mercury and Hunter River General Advertiser Wed 22 Apr 1846 Page 2

<https://trove.nla.gov.au/newspaper/article/680720>

⁷ The Maitland Mercury and Hunter River General Advertiser Tue 19 May 1857 Page 1

<https://trove.nla.gov.au/newspaper/article/18639163>

Mrs Clift entertained a great deal and the following is an early article showing a party held for young children:

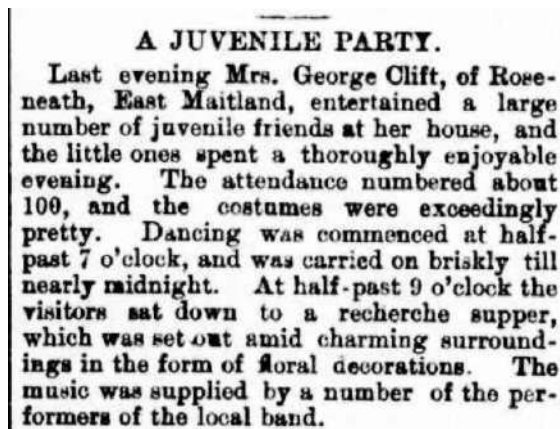


Figure 14 Roseneath - Article

source: trove Newcastle Morning Herald and Miners' Advocate Fri 21 Jun 1895
<http://trove.nla.gov.au/newspaper/article/133229019>

George Clift died in June 1912 and his will was granted in probate to Kezia Jane Clift (widow), George Percival Clift (grazier) and Albert Thomas Young of Breeza, (overseer).

One of the latest adverts shows the Clift family were still in residence in 1937 and offered for sale in 1938:

Trousseau Tea Party

*Mrs. L. Clift and her daughter, Miss Barbara Clift of 'Roseneath,' East Maitland, entertained a large party of their friends this week at a 'trousseau tea party.'*⁸



Figure 15 Roseneath – Article

source: Trove - The Maitland Daily Mercury Wed 23 Nov 1938
<http://trove.nla.gov.au/newspaper/article/125715924>

⁸ The Maitland Daily Mercury Thu 14 Oct 1937 Page 3
<https://trove.nla.gov.au/newspaper/article/126511039>

It was purchased by the "Solman" family and opened as a Guest House in 1939:

WANTED, competent General. No washing. Other help kept. Sleep in. £1 per week. Mrs D. Solman, Roseneath Guest House, East Maitland.⁹



Figure 16 9 Day St "Roseneath", East Maitland, NSW [1968]

source: University of Newcastle Living Histories - Athel D'Ombra Collection
<https://livinghistories.newcastle.edu.au/nodes/view/14792>



Figure 17 House at back of Roseneath, East Maitland, NSW, 1972

source: University of Newcastle Living Histories - Athel D'Ombra Collection
<https://livinghistories.newcastle.edu.au/nodes/view/12625>

⁹ Singleton Argus Mon 15 May 1939 Page 3
<https://trove.nla.gov.au/newspaper/article/81948105>



Figure 18 "Roseneath", East Maitland, NSW [1978]
source: Trove – Newcastle Region Libraries – Jack Sullivan
<https://trove.nla.gov.au/work/235139755>

NOTICE:

*Commonwealth of Australia Gazette. General Tue 21 Oct 1980 [Issue No.G42] Page 50
COMMONWEALTH OF AUSTRALIA Australian Heritage Commission Act 1975
NOTICE OF ENTRY IN THE REGISTER OF THE NATIONAL ESTATE*

*Maitland City
'Roseneath' (Victoria Hotel) 9 Day Street, East Maitland¹⁰*

Although listed as a Heritage Item in Schedule 5 there is no further information within NSW State Heritage Inventory for this property.

¹⁰ <https://trove.nla.gov.au/newspaper/article/240772534/25948149>

2.4 Former Post Office and Stables – 18 Day Street – State Heritage Item I31

The former Post Office and stables were built in 1870.



Figure 19 East Maitland Post Office, N.S.W. – 1930 by Fred Harvey

source: Picture Maitland

<https://www.flickr.com/photos/98887654@N05/44390544712>

The image below also shows the East Maitland Courthouse pointed out with the green arrow:



Figure 20 Old Post Office, East Maitland, NSW, Australia, 1973

source: University of Newcastle Living Histories – Athel D-Ombrain Collection

<https://livinghistories.newcastle.edu.au/nodes/view/14650>

NSW Heritage Inventory

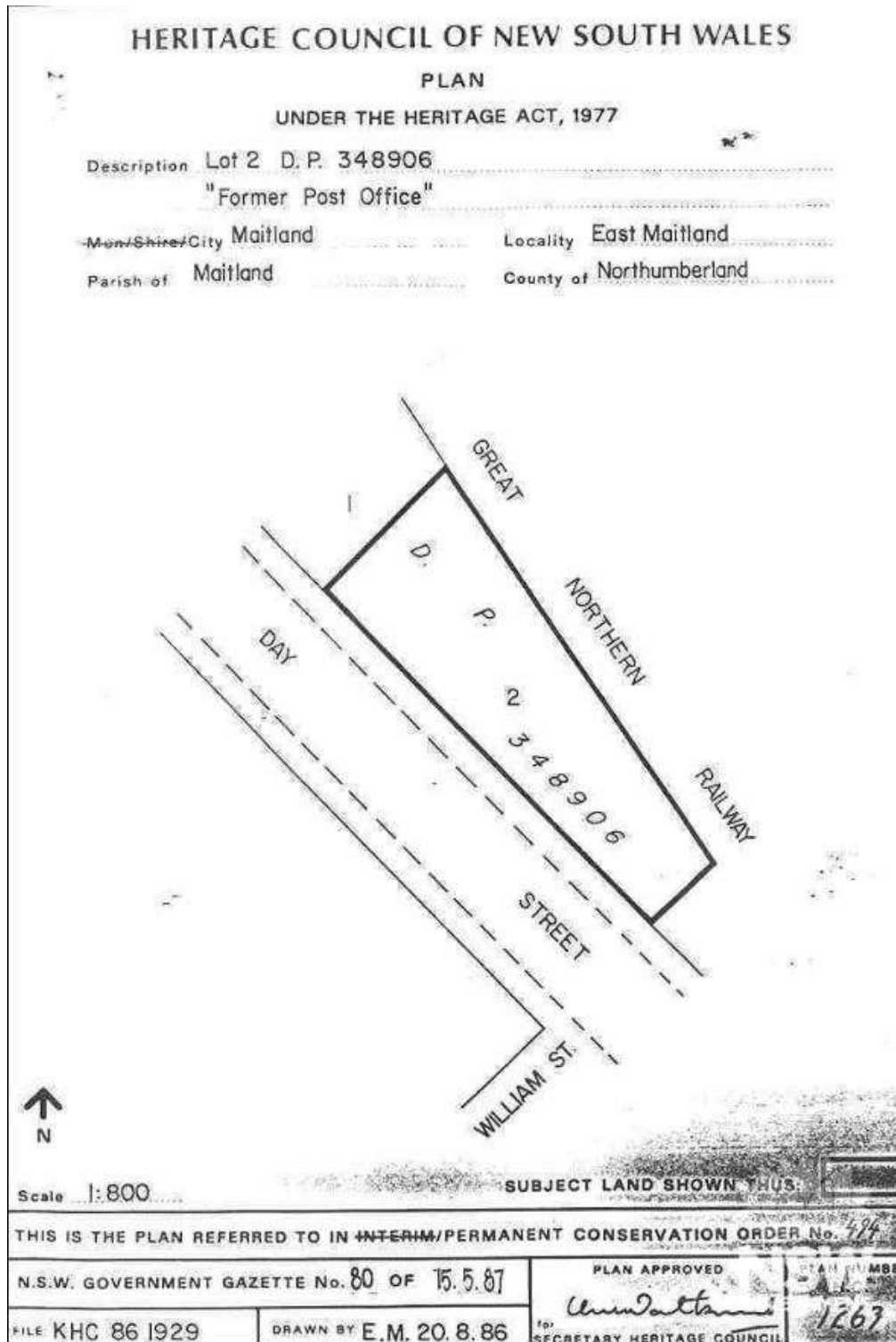


Figure 21 Post Office & Stables (former)

source: NSW Heritage Inventory

<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=5045608>

Statement of Significance

The former East Maitland Post Office is a fine example of a simple symmetrical but elegantly proportioned and detailed public building and comes from the office of one of the most important Government Architects, James Barnet. It is an impressive free standing building on a large site which together with its stables building provides important evidence of the history of the postal service in New South Wales. (Branch Managers Report 28 July 1986)

Physical Description

The post Office is a two storied, rendered brick, Victorian Italianate building of symmetrical design with a single storey timber verandah having a curved iron roof which returns at the sides to form single storey office wings. The main roof is hipped in form and of corrugated iron, embellished at the eaves by timber brackets and moulded string course. There is a sandstone paved footpath at the street frontage. The site also includes a garage which was formerly a stable and a detached toilet block.(Branch Managers Report 28 July 1986)

Historical Notes or Provenance

Constructed c.1870 the former post office was designed in the office of the Colonial Architect James Barnet.

The building ceased to be used as a post office on 3 August 1971 and was then used as a recreation and meeting room for the Maitland Branch of the Australian Postal Institute.

On 12 May 1986 the Heritage Council was advised by the Department of Local Government and Administrative Services that the property was surplus to the requirements of the Australian Postal Commission and that it intended to sell the property.

A Permanent Conservation Order was placed over the site on 15 May 1987. It was transferred to the State Heritage Register on 2 April 1999.

2.5 East Maitland Railway Station Group –State Heritage Item I19



Figure 22 East Maitland Railway Station

source: pinterest

<https://www.pinterest.com.au/pin/799740846299818870/>

East Maitland Railway Station in the early to mid 20th Century The junction to the right of the station is the line to Morpeth branch, where a train is coming in.¹¹

Construction began in 1914 and the image below shows the Heritage map of area and also shows the subject property pointed out with the green arrow with “Roseneath” diagonally opposite and the Hunter River Hotel adjacent:

¹¹ Pinterest

<https://www.pinterest.com.au/pin/799740846299818870/>



Figure 23 East Maitland Railway Station group
source: NSW State Heritage Inventory
<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=5012002>



Figure 24 East Maitland Railway Station group – July 2013

source: NSW State Heritage Inventory

<https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=5012002>

Physical Description

COMPONENTS

- Station building
- 1914 brick island platform building A9 standard branch line station (parcels office)
- 1914 brick booking office
- 1914 brick platform faces
- brick 1914 subway 1914 (RNI)

STATION BUILDING

This is a brick A9 standard island platform building design of 7 bays with cantilevered awnings to both platform faces situated on a curved platform. The planning is linear with parcels office, booking office general waiting room ladies room ladies toilet and male toilets on the platform. The design was introduced in 1913. The structure has a gable roof clad in corrugated iron, rendered bracket supports and string courses around the building. The structure is impressive and dominates the local landscape (RNI).

BRANCH LINE STATION BUILDING

This is a small A1 design waiting shed on a curved platform comprising an open waiting room, gable roof clad in corrugated iron with the same detailing as the main building. Detailing is standard (RNI).

BOOKING OFFICE

This is a one off building with standard detailing. It contains two ticket booths and ticket office room. It has the same form as the station buildings with gable roof and cantilevered awning over the waiting passengers. It fronts the street and is located obliquely to the other buildings but adjacent to the subway. There is no cover between the booking office and the station buildings (RNI).

SUBWAY

This is a brick lined access to the main platform and is of standard brick detailing with timber rails around the platform opening (RNI).

LISTING BOUNDARY

The listing boundary is the far side of the tracks on the western side, crossing the tracks on the Maitland end approximately 10m beyond the booking office, crossing the carpark and joining the near end of the branch line platform, crossing the former tracks, running parallel with the platform, then turning west at the far end of the platform continuing to the Newcastle end of the main platform (RNI).

Historical Notes or Provenance

The East Maitland railway precinct is located on the Main North line, which runs from Sydney and extends as far as Wallangarra on the Queensland border. The Main North Line (formerly known as the Great Northern Railway) runs through the Central Coast, Hunter and New England regions. The line was the original main line between Sydney and Brisbane, however this required a change of gauge at Wallangarra. The line is now closed north of Armidale, and the main route between Brisbane and Sydney is now the North Coast line.

West Maitland (now known as Maitland) was settled by farmers from the early 1800s. By the mid-1820s the population of West Maitland had reached 400, however, due to the threat of flood an alternative site for the town was found on a nearby ridge, which became East Maitland and was divided from West Maitland by Wallis Creek. East Maitland was established as a government town to handle the administrative affairs of the local region and was therefore properly surveyed and planned. Sir Thomas Mitchell devised the town plan to encourage growth, with government services such as the courthouse and post office located there and the gaol completed in 1841. During the middle of the 19th century, the three towns of East Maitland, West Maitland and Morpeth dominated the affairs of the Hunter Valley. Administrative and government functions, commercial activities and port and trade activities were controlled from East Maitland, West Maitland and Morpeth, rather than Newcastle (Kass, 2005: 23).

Following the completion of the first railway from Sydney to Parramatta Junction in 1855, proposals for the first railways to the other parts of NSW were driven primarily by pastoral communities seeking improved transport for their produce from inland centres such as Goulburn, Bathurst, Singleton and Muswellbrook (Upper Hunter). Early additions to the Great Northern Railway included Victoria Street to Maitland (opened 1858) and Maitland to Singleton (1863). These were followed by extensions to Muswellbrook in 1869, Aberdeen in 1870 and to Scone in 1871 (Rappoport, 2001; SRA, 1993).

The East Maitland railway precinct has an unusual arrangement and a slightly confusing history, with four separate East Maitland railway precincts identified in the 'How and Why of Station Names' (SRA, 1993), each one differentiated by location and date. The original East Maitland station opened on 5 April 1857 at what is now Victoria Street. The first East Maitland station closed in March 1858 but was reopened as 'Victoria Street' in 1877. The second East Maitland station opened on 27 July 1858 near the courthouse and at a location not far from the existing station precinct. The third East Maitland station opened as 'Morpeth Junction' in 1864, changed to 'East Maitland' in 1879 and closed in 1914 (SRA, 1993; Forsyth, 1985).

On 14 June 1914, the third station at East Maitland closed and was replaced by a fourth station at what is now East Maitland railway precinct. The Morpeth line junction was then altered to join the Northern line closer to Newcastle. A separate platform was established on the branch line to handle trains to and from Morpeth, this being adjacent to the main line platforms but at a lower level. The extant buildings on the former Morpeth branch line are believed to have been constructed in c1914 (Forsyth, 1985).

In the 1920s, Morpeth's traffic declined due to changes in the transportation of wool and improvements to wharf facilities at Newcastle. The line closed to traffic on 31 August 1953 and was officially closed by Parliament in December of the same year (Forsyth, 1985). The station building and other infrastructure located on the main Newcastle to Maitland line at East Maitland remain as operational facilities with regular passenger services.

Extant items include those on both the Newcastle to Maitland line and on the adjacent closed Morpeth line. These include the platform building (1914), brick booking office (1914), parcels office (branch line station), brick platform faces (1914), brick subway, station lights, fences and station signs. The building believed to be the parcel office may have been the former station building on the earlier alignment of the Morpeth branch line (before construction of the fourth East Maitland railway station in 1914) (SRA/ORH, 2009).

East Maitland Railway Precinct

Statement of Significance

The East Maitland railway precinct is of significance as a mostly intact, early 20th century railway location that includes a standard 'A-type' brick station building as well as a group of similar Federation period station buildings associated with the former branch line to Morpeth (now closed). The extant railway buildings, all dating from c1914, contribute to the townscape of East Maitland and reveal the development of the Great Northern Railway and the former use of the branch line to Morpeth.¹²

¹² <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=4280006>

3. PLANNING AND HERITAGE CONTEXT

3.1 Maitland Local Environmental Plan 2011

The subject site is within the **East Maitland Heritage Conservation Area** and is not a Heritage Item however is within the vicinity of a number of heritage items as follows:

Schedule 5 Environmental Heritage

Suburb	Item name	Address	Property description	Significance	Item no
East Maitland	East Maitland Railway Station group	Main Northern Railway		State	I19
East Maitland	"Roseneath"	9 Day Street	Lot 200, DP 873102	Local	I30
East Maitland	Former post office and stables	18 Day Street	Lot 2, DP 348906	State	I31

The following is an extract from the LEP that is relevant to this proposal:

Objectives

5.10 Heritage conservation

- (4) Effect of proposed development on heritage significance The consent authority must, before granting consent under this clause in respect of a heritage item or heritage conservation area, consider the effect of the proposed development on the heritage significance of the item or area concerned. This subclause applies regardless of whether a heritage management document is prepared under subclause (5) or a heritage conservation management plan is submitted under subclause (6).
- (5) Heritage assessment The consent authority may, before granting consent to any development—
 - (a) on land on which a heritage item is located, or
 - (b) on land that is within a heritage conservation area, or
 - (c) on land that is within the vicinity of land referred to in paragraph (a) or (b),require a heritage management document to be prepared that assesses the extent to which the carrying out of the proposed development would affect the heritage significance of the heritage item or heritage conservation area concerned.

In compliance with Clause 5 (b and c), a Statement of Heritage Impact has been prepared to assess the impact that the proposed development will have on the significance of the conservation area and heritage items in the vicinity.

3.2 Maitland Development Control Plan 2011

Part C.4 – Heritage Conservation

1. Introduction

1.1 Preamble

Maitland's environment has value to us all as links to the past. Heritage items, Conservation Areas, archaeological sites and historic artefacts individually and collectively have profound importance. They provide a source of community identity, evidence of the evolution of society's values, the impetus and inspiration for new ideas or the revival of the old. They also provide a wonderful source of aesthetic satisfaction and are an increasingly important economic resource.

An understanding of Maitland's historic environment provides important background information to anyone considering development that may impact on a heritage item or a heritage conservation area. This information is attached as an Appendix.

1.2 Application

This chapter applies to all heritage items, including heritage conservation areas, to which clause 5.10 in the Maitland LEP 2011 applies.

Matters relating to Aboriginal heritage significance are addressed in clause 5.10(8) in the Maitland LEP 2011, but are not specifically addressed in this chapter.

Clause 5.10 specifies when a development application is required. Where clause 5.10(3) stipulates when consent is not required, it is the responsibility of the applicant/owner to ensure that the proposed works satisfy the requirements of this subclause before undertaking any work.

Chapter B: Vegetation Management in this DCP also applies to any trees listed as a heritage item or any trees located within a heritage conservation area.

Specific guidelines relating to the Heritage Conservation Areas are contained in Part E: Heritage Conservation Areas. Where relevant, Part E should be read in conjunction with this Chapter and B: Vegetation Protection.

(Note: A number of terms relating to heritage are defined in the Maitland LEP 2011 for the purposes of interpreting clause 5.10).

1.3 Purpose

To provide detailed explanation and guidance on Council's requirements in relation to heritage design and conservation matters.

1.4 Objectives

To assist owners and developers who are contemplating carrying out development that may impact on a heritage listed property or conservation area.

To promote an attractive living and working environment for the community of Maitland, which builds on its particular identity.

5. General Requirements for New Commercial Buildings in Historic Areas

In addition to the above, new development in commercial precincts within Conservation Areas, or that adjacent to a Heritage Item should take into account the following issues.

General Requirements:

1. Building Heights and Setbacks

The height of buildings should reinforce the desired scale and character of the area. Maximum building heights have been set out in Part E.3, 'Heritage Conservation Areas'.

2. Services

Service structures, and plant and equipment within a site should be an integral part of the development and should be suitably screened buildings and should not be built out.

3. On – Site Loading and Unloading

Facilities for the loading and unloading of service vehicles should be suitably screened from public view.

4. Design of Car Parking areas

Car parking areas should be located and designed to:

- a. *provide landscaping where practicable to shade parked vehicles and screen them from public view.*
- b. *provide for access off minor streets, and for the screening from public view of such car parking areas from surrounding public spaces and areas.*

5. Car Park Structures Should:

- a. *incorporate a façade designed to complement adjoining buildings in an urban context.*
- b. *be setback from the street frontage and out of view if possible.*

6. Roof Form, Parapet and Silhouettes

- a. *In Commercial areas, it is the consistency of parapets which make a significant contribution to the architectural character of an area.*
- b. *Where the prevailing pattern of roof forms assists in establishing the character of a townscape, new roof forms should seek to be compatible with the shape, pitch, and materials of adjacent buildings.*
- c. *Parapet heights and articulation should be compatible with earlier surrounding buildings.*
- d. *Lightweight materials such as ribbed coloured metals should not be used on vertical wall or parapet surfaces.*
- e. *New verandahs should be based on design principles of traditional verandahs with sloping roofs galvanised iron and regularly spaced columns.*

6. New Development in the Vicinity of Heritage Items

In addition to the matters raised previously, the following principles should be given particular attention when considering new development in the vicinity of heritage items.

Objective:

General Requirements:

- a. Development in the vicinity of listed heritage items should respect and complement the built form character of those items in terms of scale, setback, siting, external materials, finishes and colour.*
- b. New development should have regard to the established siting patterns of the locality.*
- c. New development should generally be set back from the building line of the adjoining or adjacent heritage item.*
- d. The sensitive selection of materials, colours and finishes is important in terms of achieving compatibility with the heritage items.*
- e. Height and scale of new buildings should not obscure or dominate an adjoining or adjacent heritage item.*
- f. Development in the vicinity of a heritage item may be contemporary in design.*

Part E Special Precincts

E.3 Heritage Conservation Areas

The approximate location of the subject property is pointed out with the green arrow in the image below:

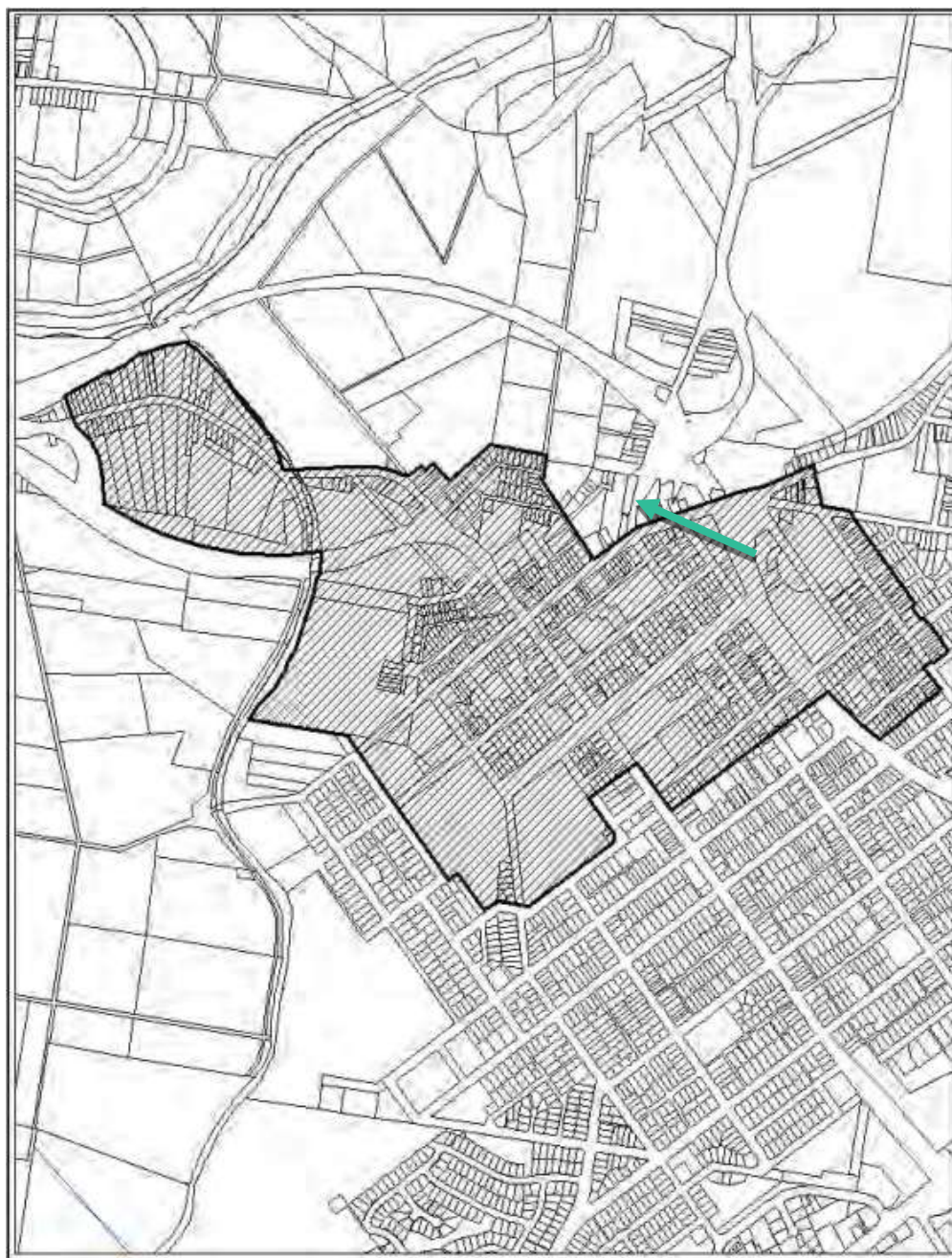


Figure 25 Locality Plan – East Maitland Conservation Area
source NSW State Heritage Inventory

3. East Maitland Heritage Conservation Area

1.3 Conservation Policies

What to Keep:

- *Retain significance of the area as a relatively rare example of a town based on government/administrative functions;*
- *Retain formal street plantings and footpaths in and around William and Banks streets;*
- *Retain the landscape setting of the major administrative buildings such as the Courthouse and formal nature of original street layout;*
- *Retain street widths of original townships and terminations of major vistas at parks;*
- *Retain scale of original residential development within the limits of original township;*
- *Retain original subdivision pattern, lot sizes and building setbacks.*
- *Original layout of sandstone kerb and guttering.*

What to Encourage:

- *Generally low density, residential development retaining existing subdivision layout;*
- *High quality of new and infill commercial design through better guidelines and more stringent controls.*

What to Avoid:

- *Large scale medium density development on large lots or at the rear of existing dwellings on larger lots;*
- *Re-subdivision of large residential lots;*
- *Inadequately controlled expansion of "Support Business" and "Special Business" uses in the Heritage Conservation Area.*

4. STATEMENT OF SIGNIFICANCE

4.1 Statement of Significance: East Maitland Heritage Conservation Area

East Maitland's historic significance is in its surviving record of the urban growth of Maitland. It is a relatively rare example of a town with origins based primarily on government functions, with links to the convict period and early immigration (Caroline Chisholm House).

Its government functions, continued in the ongoing use of the Gaol (over almost 150 years), Courthouse and Lands Office and in the preservation of the former police buildings and Post Office, contribute to both historic and social significance.

The Heritage Conservation Area's aesthetic significance is derived from its collection of residential, government, institutional and commercial buildings of all its periods of historic growth and their visual inter-relationship, in particular the strong axial composition based on the prominent hillside location of the Court House and Stockade Hill.

5. PHYSICAL DESCRIPTION

5.1 Context

- a. The subject site is located at 12-14 Day Street, East Maitland and is accessed from Day Street.
- b. Day Street runs generally north to south along with the railway line to the east.
- c. The subject site is an unusual block as an isolated residential site on the north-eastern side of Day Street with residential land occupying the majority of land to the south-western side of Day street.
- d. The neighbouring site to the north is the Hunter River Hotel site within a B4 Mixed Use zone whilst the neighbouring site to the south is vacant and zoned RE1 Public Recreation, to the south of that is the East Maitland Post Office and to the north-east is East Maitland Station.
- e. A large batter behind the rear boundary of the site creates a vegetated corridor between the site and the railway line which is substantially higher than the subject property.

5.2 12-14 Day Street, East Maitland

- a. The existing site is occupied by a single weatherboard residential dwelling on a raised subfloor of masonry piers.
- b. A number of trees on the north-western boundary separate the site from the adjacent hotel carpark.
- c. Views to and from the site are relatively restricted by surrounding development and land formation as well as existing vegetation.
- d. There is a mix of fencing surrounding the site, with open steel mesh fencing to the front boundary and profiled steel fencing to the south-eastern boundary. The rear boundary is partially fenced in Colorbond whilst the north-western boundary to the hotel carpark consists of a concrete retaining wall with remnant timber fence elements on top.

5.3 Summary Condition and Fabric

Exterior



The front of the dwelling presenting to Day Street. **Overall Condition: Poor**



Looking towards the rear of the site with the dwelling visible to image left.



The rear of the dwelling presenting towards the railway line.



The rear of the dwelling showing brick piers and fibre-cement weatherboard cladding.



Rear deck area showing fibre-cement weatherboard cladding and aluminium windows.



Subfloor space showing particle board flooring over timber bearers and joists on brick piers.



South-eastern elevation



Front elevation showing brick subfloor with fibre-cement weatherboard dwelling over.



Front elevation showing brick subfloor with fibre-cement weatherboard dwelling over.



North-western elevation as viewed from the adjacent hotel.

Interior



Open plan kitchen through to front dining and living space. **Overall Condition: Poor**



Dining space looking towards the kitchen.



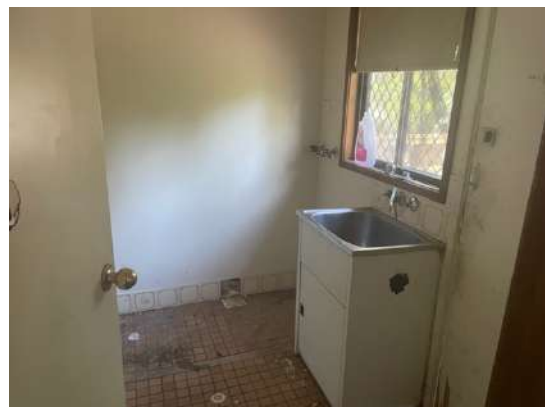
Bedroom



Living space



Living space looking looking towards dining area



Laundry area with rear door to back deck.

6. THE PROPOSAL

6.1 Introduction

- a. The proposal is for demolition of the existing dwelling and clearance of the site ready for a new commercial development.
- b. The proposal is for the construction of a four unit commercial development with associated parking and hardstand. The proposal shall include mezzanine levels within a two storey volume.
- c. The proposal will include alterations to existing landscaping and natural ground levels in order to manage stormwater and flooding appropriately.
- d. The proposal will include new boundary fencing and generally improved presentation of this site which has fallen to disrepair and neglect.

6.2 Documents

This statement was prepared in conjunction with the DA drawings. It is our professional practice to review all design work before finalising the Statement of Heritage Impact and to recommend changes if required in order to make the proposal more appropriate in the heritage context.

Documents reviewed in preparation of this Heritage Impact Statement were prepared by **SORENSEN DESIGN AND PLANNING:**

Page No	Description	Issue	Date
SHEET 1	INDEX		25/01/23
SHEET 2	STREET VIEW		
SHEET 3	SURVEY		
SHEET 4	SITE PLAN		
SHEET 5	GROUND FLOOR PLAN		
SHEET 6	FIRST FLOOR PLAN		
SHEET 7	STREET SECTION AND ELEVATION		

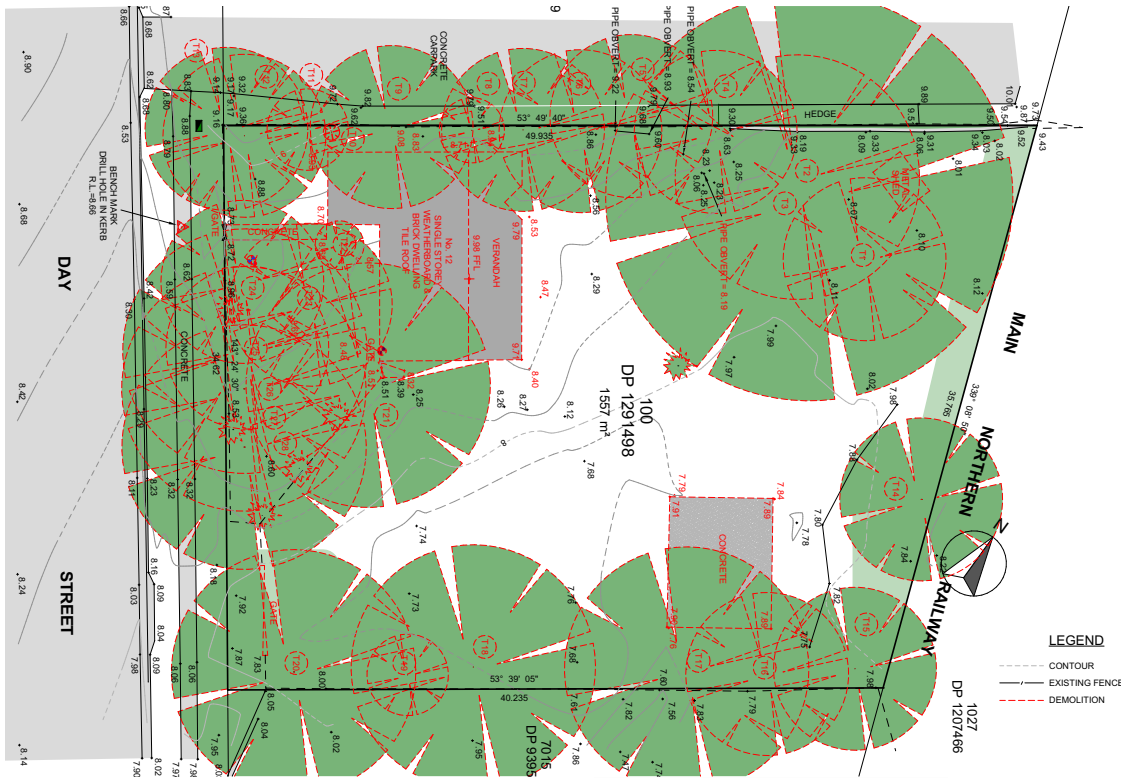


Figure 26 Survey source SORENSEN DESIGN & PLANNING

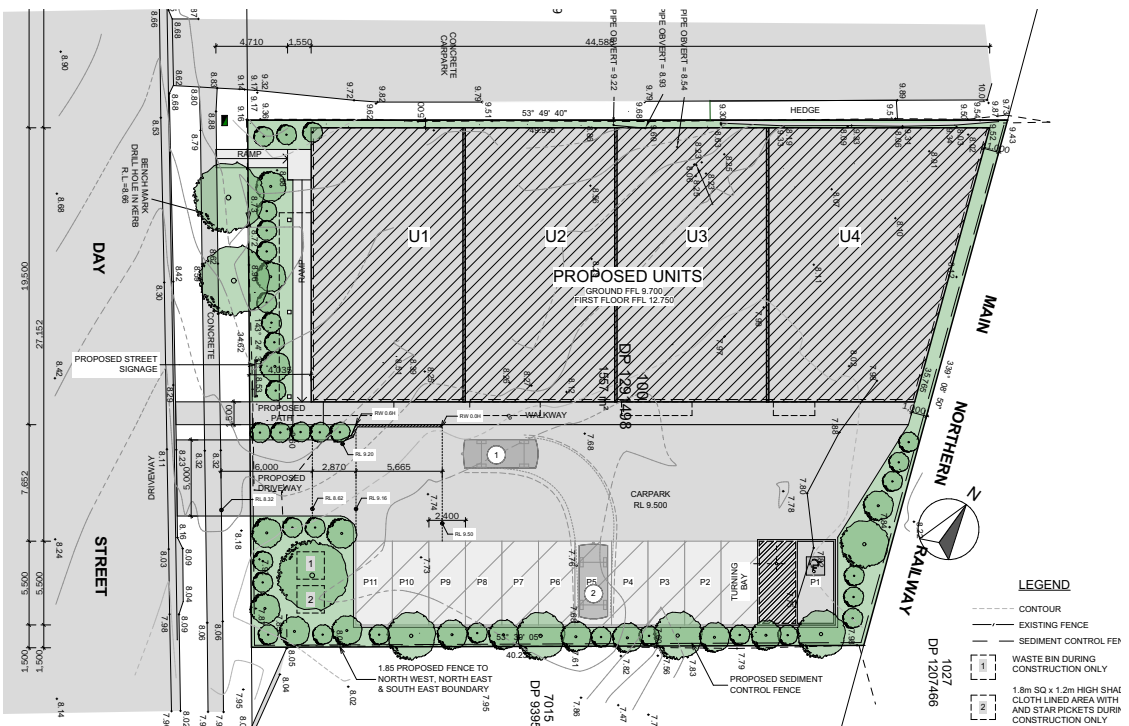


Figure 27 Proposed Site Plan source SORENSEN DESIGN & PLANNING

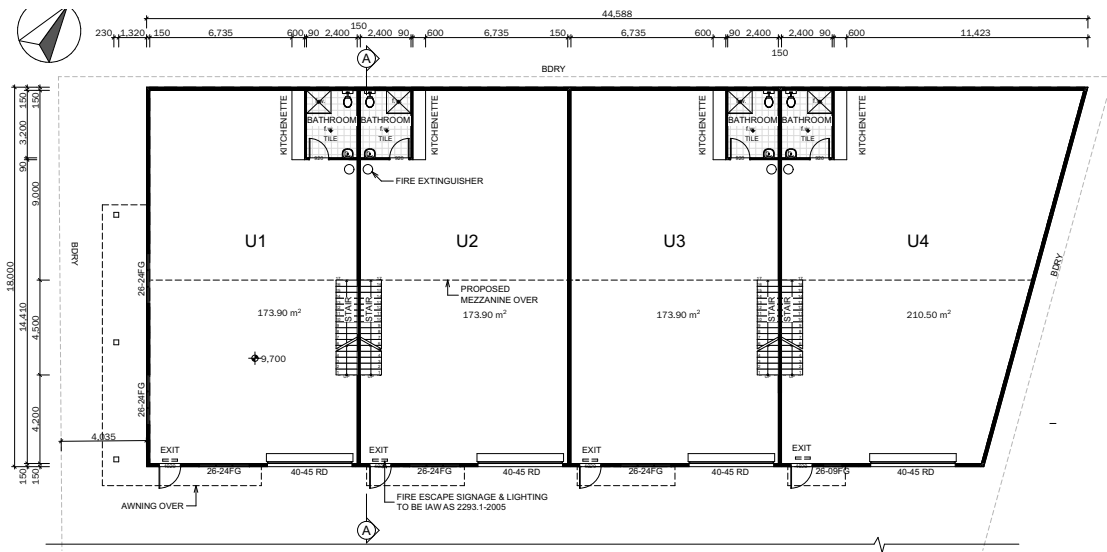


Figure 28 Proposed Ground Floor Plan
source SORENSEN DESIGN & PLANNING

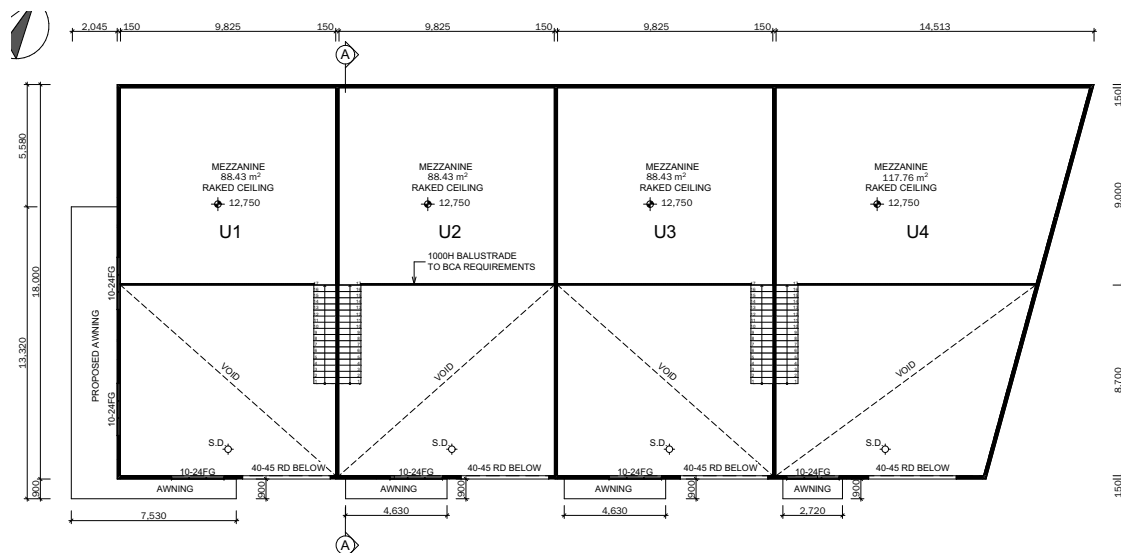


Figure 29 Proposed First Floor Plan
source SORENSEN DESIGN & PLANNING



Figure 30 Proposed Street Section
source SORENSEN DESIGN & PLANNING

7. ASSESSMENT OF IMPACT

7.1 Identification of Impacts

- a. Potential impacts arising from the proposed development include:
 - impact of proposed new work on the East Maitland Heritage Conservation Area;
 - impact of the proposed work on Heritage Items in the vicinity
- b. This Statement of Heritage Impact analyses the extent of these potential impacts and the measures taken to mitigate any negative impacts. The assessment of impact is based on the known heritage significance of the Conservation Area and the policies and guidelines of the relevant planning instruments.

7.2 Assessment of Impacts

Demolition of Existing Dwelling

- a. The original dwelling on the site was demolished many years and remnant elements are unlikely to remain due to disturbance from subsequent development of the site.
- b. The existing dwelling is not of any significance as a later structure on the site and its demolition will not be detrimental to the Conservation Area.
- c. The existing building is in particularly poor condition and presents negatively to the street. At present it makes no contribution to the Heritage Conservation Area and its replacement with a suitably designed infill development is considered positively.

Proposed Infill Commercial Development

- d. The proposed commercial development of the site is suitable despite its present zoning due to its isolated position on this north-eastern side of Day Street. The use of the site has been historically residential however a commercial development in keeping with its immediate commercial context would be an appropriate redevelopment opportunity that would have minimal impact on the special qualities of the Heritage Conservation Area.
- e. The proposed setback of the building from the street is 4m which is in excess of the adjacent hotel which is built to boundary. This allows some hierarchy and reduction in building dominance with progression from north to south along Day Street, with the vacant land adjacent to the south and the East Maitland Post Office building which has a considerable setback.
- f. The scale of the proposal has been suitably considered, limiting the height of the proposal to below that of the significant buildings in the vicinity, including Roseneath opposite and East Maitland Station to the rear. The proposed parapet of this infill development shall terminate at the approximate gutter level of both buildings which is positive.

- g. The proposed new commercial building is a larger structure, however turning its width perpendicular to the street assists in mitigating its impact on the nearby Roseneath which shall remain a more dominant building with a wider, greater, street presence.
- h. The proposed infill of the site creates additional mass that would usually be avoided however the flood zoning of the site makes this unachievable. As a result, careful siting of new vegetation has been considered in order to mitigate the impact of this raised level as much as possible.
- i. The proportions and rhythm of the nearby Heritage Items, including Roseneath and the East Maitland Post Office in particular have been referenced in the articulation of the proposal. Breaking its mass into smaller, vertically proportioned sections which references the rhythm of the two storey verandah of Roseneath. A horizontal break in the building was introduced during design development stages of the project to further respond to its context.
- j. The form and massing of the proposal has been considered in order to remain sympathetic to the presentation of the Conservation Area. The predominant rectilinear form of surrounding buildings has been referenced, with vertically proportioned glazing utilised to reduce the apparent mass of the structure suitably. Pitched roofs were considered in order to respond to the predominant pitched roof form of the context however were dismissed due to the additional mass and height that would result, instead the proposal limits building mass to the gutter level of nearby structures.
- k. Landscaping has been developed in order to replace trees that have been removed as part of this proposal and to further improve the presentation of the proposal to the street and adjacent vacant land. The site is currently heavily vegetated and this should not be entirely lost as a result of the proposed development. Street tree planting should be considered in order to provide additional large tree planting in front of the proposal.
- l. The proposal is clearly contemporary however detailing has been careful to respond to the colours and materials of significant buildings in the vicinity which include painted and unpainted mixed reds/browns brickwork whilst remaining commercial in presentation. Brickwork should consist of standard full size bricks and not imitation brick cladding.
- m. The north-western elevation is a 45m long, unarticulated painted concrete wall. Whilst this wall presents towards an existing carpark area, further development and articulation should be provided due to its visibility from the public domain and across the carpark from Day Street. A larger setback could also be considered in order to provide opportunity for screening plants.
- n. The presentation of doors and windows have been developed to respond to typical openings in the locality which is positive, using vertical mullions to enhance a vertical orientation.
- o. Whilst less visible, the roof shall be simple corrugated steel in a shale grey colour with a commercial half round gutter. Gutters should incorporate externally mounted brackets and not be slotted.

- p. There is a substantial area of hardstanding in order to provide necessary parking and space for safe movement of vehicles. It is recommended that parking is rearranged to enable two 1.2m vegetated islands to break up parking into 3 sets of 4 bays rather than a single continuous stretch, this would provide shade to parked vehicles and screening from public view. Similarly, providing a projection for vegetation on the south-eastern boundary would be beneficial. Consideration could be given to breaking up the length of hardstanding, measured from front to rear, with bands of a differing colour or finish texture, perhaps aligning with planted sections.
- q. Plant equipment is accommodated within each unit as well as below proposed hardstand areas in order to mitigate the impact of service and plant equipment as much as possible.
- r. The proposal is respectful of the heritage items in the vicinity, maintaining a larger scale that is appropriate to its immediate context. The natural separation between the site and the much smaller station buildings to the rear substantially mitigates potential impacts which is further compounded by the substantial change in level from the lower site to the much higher station platform.
- s. The contemporary nature of the proposal will allow it to be legibly new within an older setting, ensuring that new work does not detract from significant earlier buildings.
- t. Proposed signage has been considered, using simple freestanding signage as well as simple fixed lettering to the front elevation. Larger printed panel signs have been avoided which is positive.
- u. The proposal is compatible with the objectives of the Maitland Development Control Plan 2014 (LMDCP 2014) and will enable the character of East Maitland Heritage Conservation Area to be retained and improved.

7.3 Views and Settings

- a. The site is partially open, with no development immediately adjacent to the south whilst the western side of the street is residential in nature including a mixture of sympathetic, contributory buildings and a Heritage Item.
- b. The hotel site to the north will obscure the proposal from views from the north however with some visibility over the adjacent carpark. It is recommended that some existing trees are retained to the north-western boundary in order to mitigate the potential impact of the proposed building on the presentation of the Conservation Area. Alternatively, the design should be developed further in order to treat this side of the building as a tertiary frontage.
- c. The site has a higher visibility when looking north along Day Street and from the railway footbridge to the south. These views need to be considered and it is recommended that existing tree planting along the south-eastern and rear boundaries is maintained around the carparking areas. This will allow a reasonable transition between built development and the open site adjacent to the south and will appropriately obscure views from the south and southern end of the rail platform.
- d. Whilst the building will be positioned away from the street junction between Day and Banks Street, the view along Bank Street will be affected by tree removal along the south-eastern boundary. It is recommended that these trees are retained or that replacement planting is provided.
- e. Views to the proposal from the State significant East Maitland Station are particularly important. This is largely managed by the existing change in levels and vegetation buffer managed to the rear of the site. Existing or replacement large tree planting at the head of the driveway is important to maintain a suitable setting for the station building whilst the rear elevation has been designed to present and respond to its context.



View from the south-west looking along Banks Street. The site is partially out of frame to image left.



View from the site looking north from the footbridge. The loss of tree planting would be detrimental and as such trees should be retained or replacement planting provided.



View from the site looking north from the footbridge. The loss of tree planting would be detrimental and as such trees should be retained or replacement planting provided.

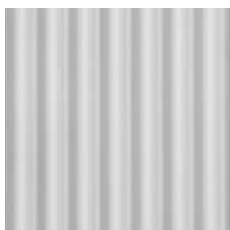


View from the southern end of the railway platform, the station building is visible to image right. The site of the proposal is obscured by trees to centre.

- f. The change in ground levels and the position of adjacent structures will increase the visual absorption capacity of the site. The extent to which the existing visual environment can reduce or eliminate the perception of the visual effects of the proposal is increased when viewed from the south and from the station as the existing development to the north and west will form a visual built backdrop for the proposal. The loss of trees will affect this and as such needs to be considered.
- g. The proposal will not sit out of place within its context with nearby Heritage Items and East Maitland Station. Appropriate form, scale and materiality manage this very well, with the building designed to be viewed essentially in the round in order to respond to its specific context.

7.4 Materials and Colours

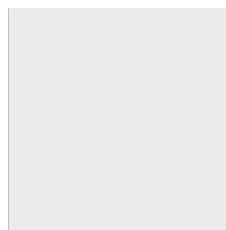
- a. The eastern elevation should be further developed and as such a colour and material selection has not yet been identified. It is recommended that articulated brickwork and colours utilised elsewhere in the scheme are used across this elevation.
- b. The proposed colours and materials are as follows or similar:



LIGHT TO MEDIUM GREY
NEW ROOFS



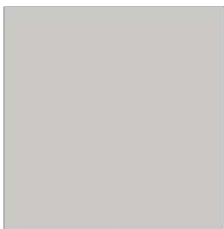
PROPOSED BRICK
WARM BROWN TONES



DULUX NATURAL WHITE
PAINTED PILLARS



CONCRETE
HARDSTAND AREAS



DULUX DIESKAU
PAINTED CONCRETE

8. RECOMMENDATIONS AND CONCLUSIONS

The following conclusions are made based on the assessment of heritage impact:

- a. The proposal is sympathetic to the character of the East Maitland Heritage Conservation Area whilst presenting legibly as contemporary infill development.
- b. The proposal will not negatively impact upon heritage items in the vicinity due to the design development of the proposal and recommendations included within this report. The proposal makes an improvement to the presentation of the site within the Heritage Conservation area over the existing dwelling which is in poor condition.
- c. The following recommendations have been made:
 - Street tree planting should be considered in order to provide additional large tree planting in front of the proposal.
 - Brickwork should consist of standard full size bricks and not imitation brick cladding.
 - It is recommended that parking is rearranged to enable two 1.2m vegetated islands to break up parking into 3 sets of bays rather than a single continuous stretch, this would provide shade to parked vehicles and screening from public view. Similarly, providing a projection for vegetation on the south-eastern boundary would be beneficial.
 - Consideration could be given to breaking up the length of hardstanding, measured from front to rear, with bands of a differing colour or finish texture, perhaps aligning with planted sections.
 - It is recommended that some existing trees are retained to the north-western boundary in order to mitigate the potential impact of the proposed building on the presentation of the Conservation Area. Alternatively, the design should be developed further in order to treat this side of the building as a tertiary frontage.
 - Trees along the south-eastern boundary should be retained or replacement planting provided.
- d. The proposal is compatible with the objectives of the Maitland Development Control Plan 2014 (LMDCP 2014) and will enable the character of East Maitland Heritage Conservation Area to be retained and improved.
- e. This report shall be read in conjunction with the final development application drawings and Statement of Environmental Effects.
- f. The final assessment is that based on the considerations within this Statement of Heritage Impact, the proposal should be approved.



Jason Penhall
CONTEMPORARY HERITAGE



Jadine Penhall

9. APPENDIX



View of the front boundary of the site from Day Street. Existing trees obscure views of the station behind.



Within the site facing to the rear boundary. Existing trees obscure views of the station behind.



Looking from the front boundary towards the south along Day Street. The former East Maitland Post Office building is visible in the distance to image left.



Looking from the front boundary of the site towards the north along Day Street. Roseneath is the dominant element to image centre.



Looking from the boundary of Roseneath and towards the subject site. Existing trees obscure views of the station behind.



View looking south along Day Street with Roseneath to image right and the larger trees to image centre marking the position of the subject site with vehicle entry to the adjacent carpark also visible.



View from within the hotel carpark towards the station. The position of larger trees to image right mark the general position of the rear corner of the site.



View from the junction of Day and Banks Street towards the subject site. East Maitland Station visible to the rear.



View from recreation land towards the site and with East Maitland Station visible to the rear.



View from John Street to the east looking towards the subject site with the station buildings visible to image centre. The proposal will not be visible from this viewpoint.



Looking from the station entrance towards the subject site. The station building is in the foreground. With the height of the proposal terminating below the gutter level of the station building, it is evident that any views of the proposal will be substantially obscured.



Looking from the station platform towards the rear of the site. Roseneath is not visible though the adjacent hotel provides a reference for the proposal which shall have an overall height roughly at gutter level.



Looking from the station platform towards the subject site. Existing wall and some vegetation lies within the existing buffer between the railway lines and the site and as such shall be retained.



Looking from the station platform towards the rear of the site. Roseneath is not visible though the adjacent hotel provides a reference for the proposal which shall have an overall height roughly at gutter level.



Looking from the station platform towards the subject site. Existing wall and some vegetation lies within the existing buffer between the railway lines and the site and as such shall be retained.